



Bethlehem Parking Authority 33 Walnut Street Parking Garage Replacement

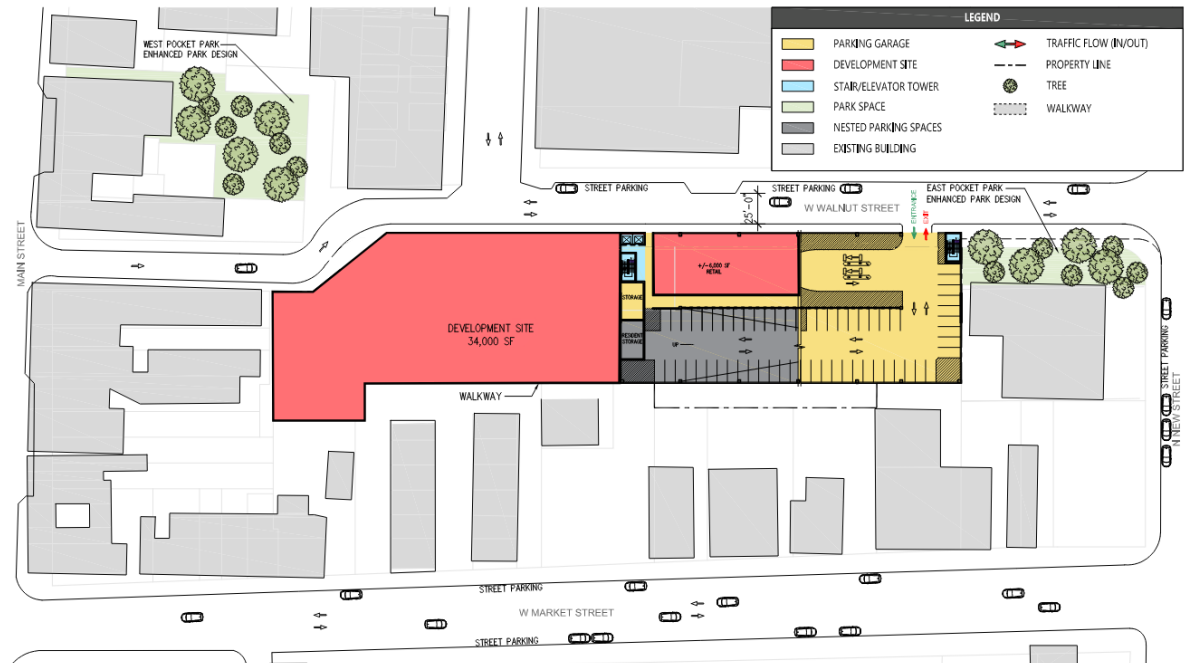
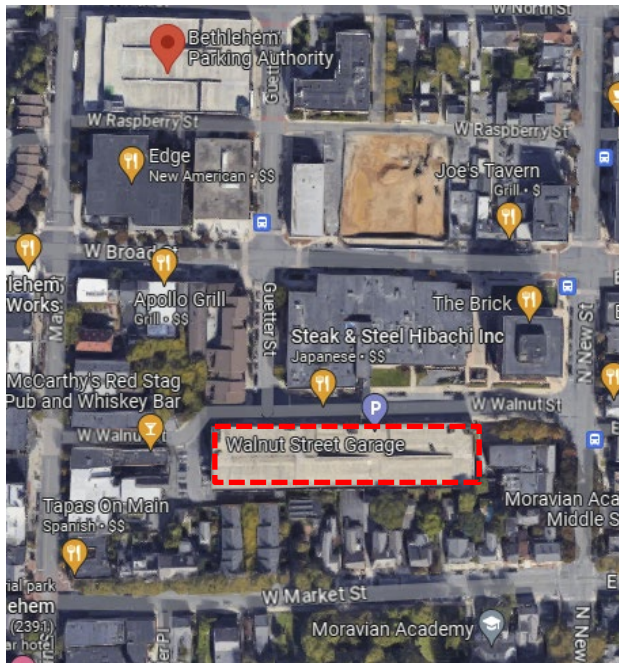
*Historic / Architectural Review
Board Meeting
Existing Garage Demolition*

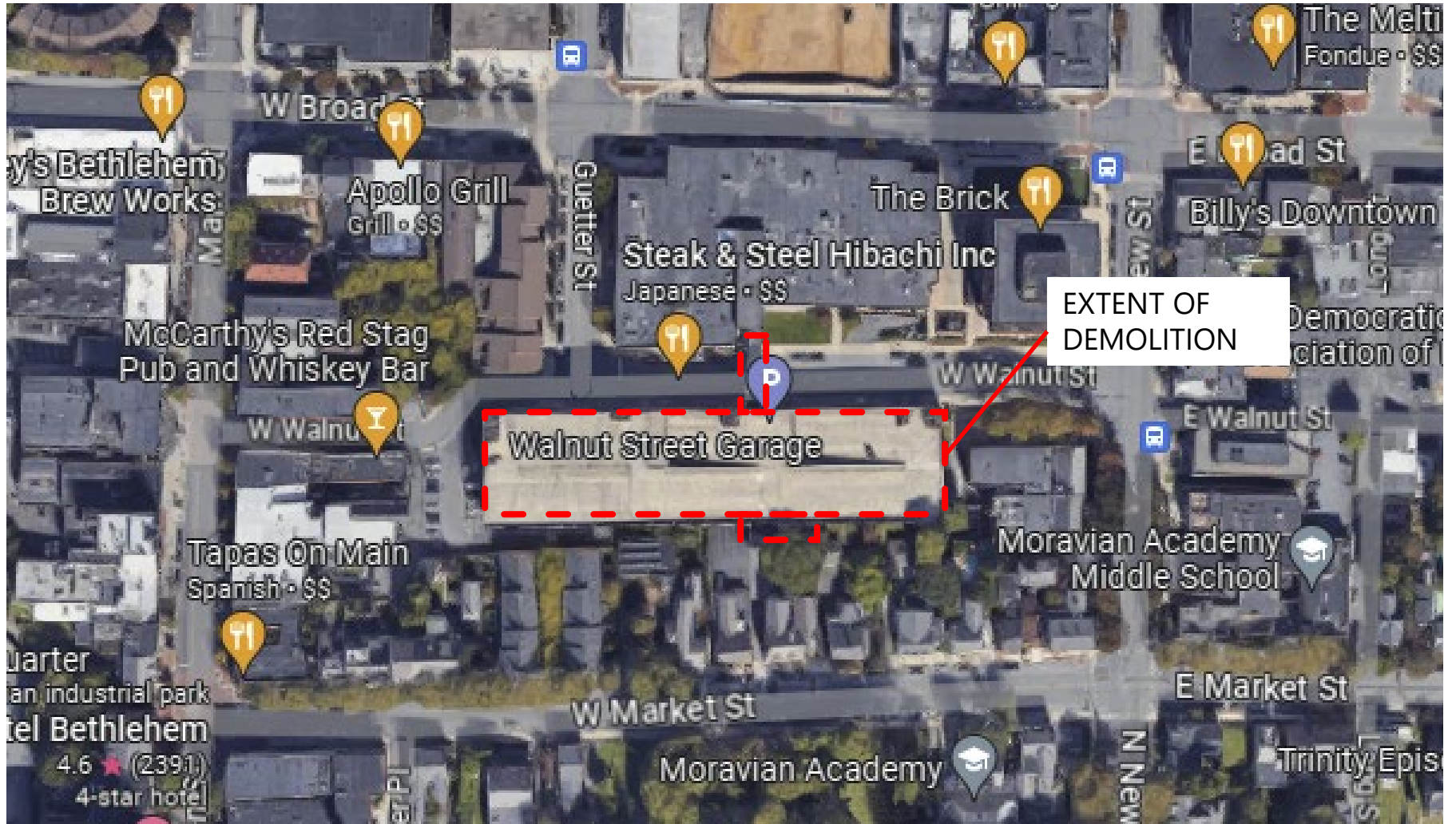
August 2, 2023

INTRODUCTION – PROJECT OVERVIEW

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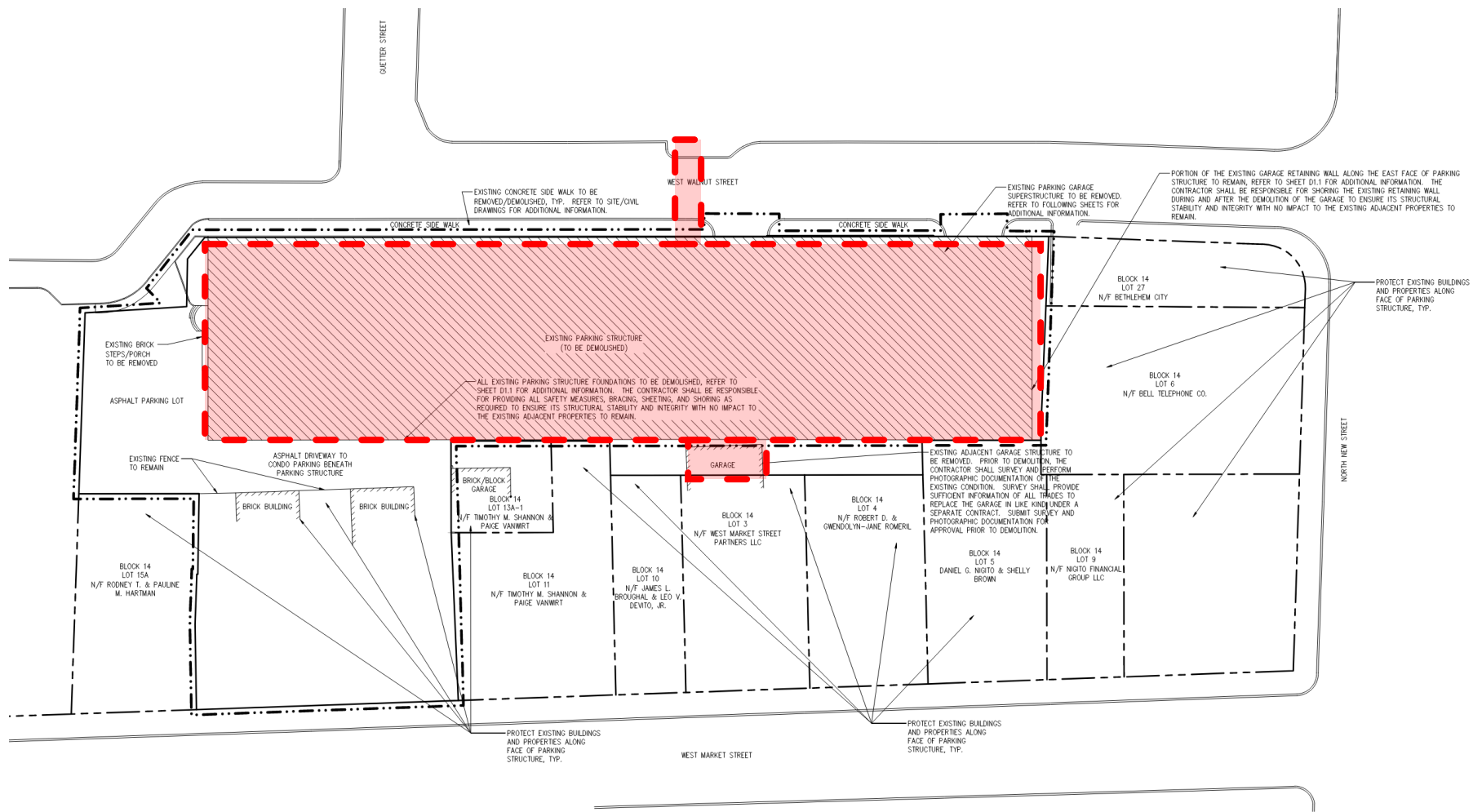
- Demolition of existing parking structure located at 33 West Walnut Street, Bethlehem, PA
- Parking facility will include core/shell design for approximately 6,000 sf of amenity space at ground tier
- West side of site - future development
- Zoning District: CB Central Business District, Downtown Bethlehem Historic District





SITE PLAN – EXISTING LOT LINE AND DEMOLITION

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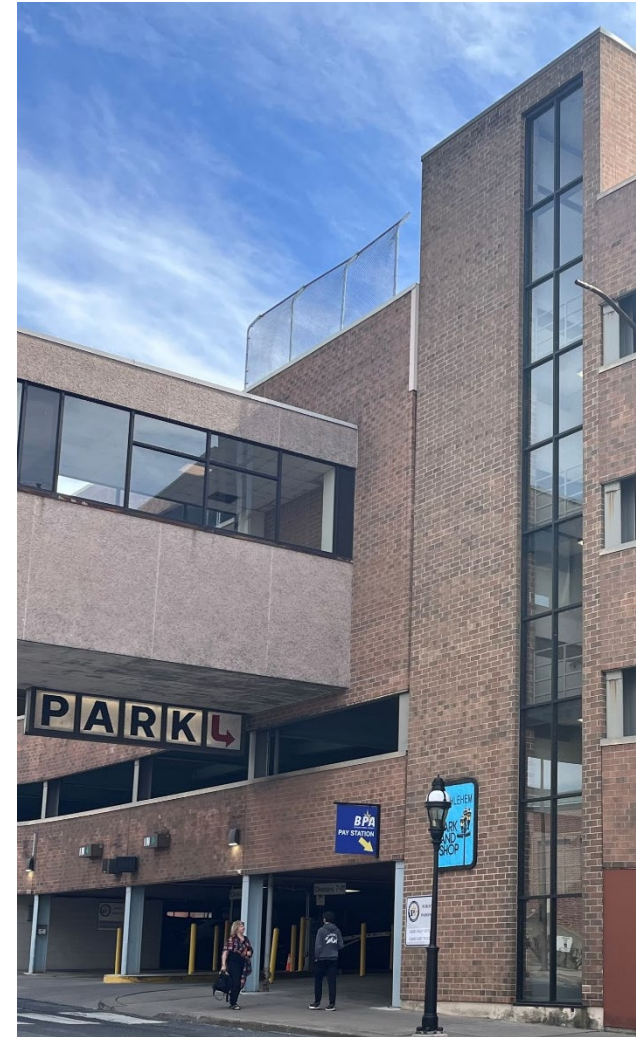
North Elevation / Walnut Street Façade



East Elevation



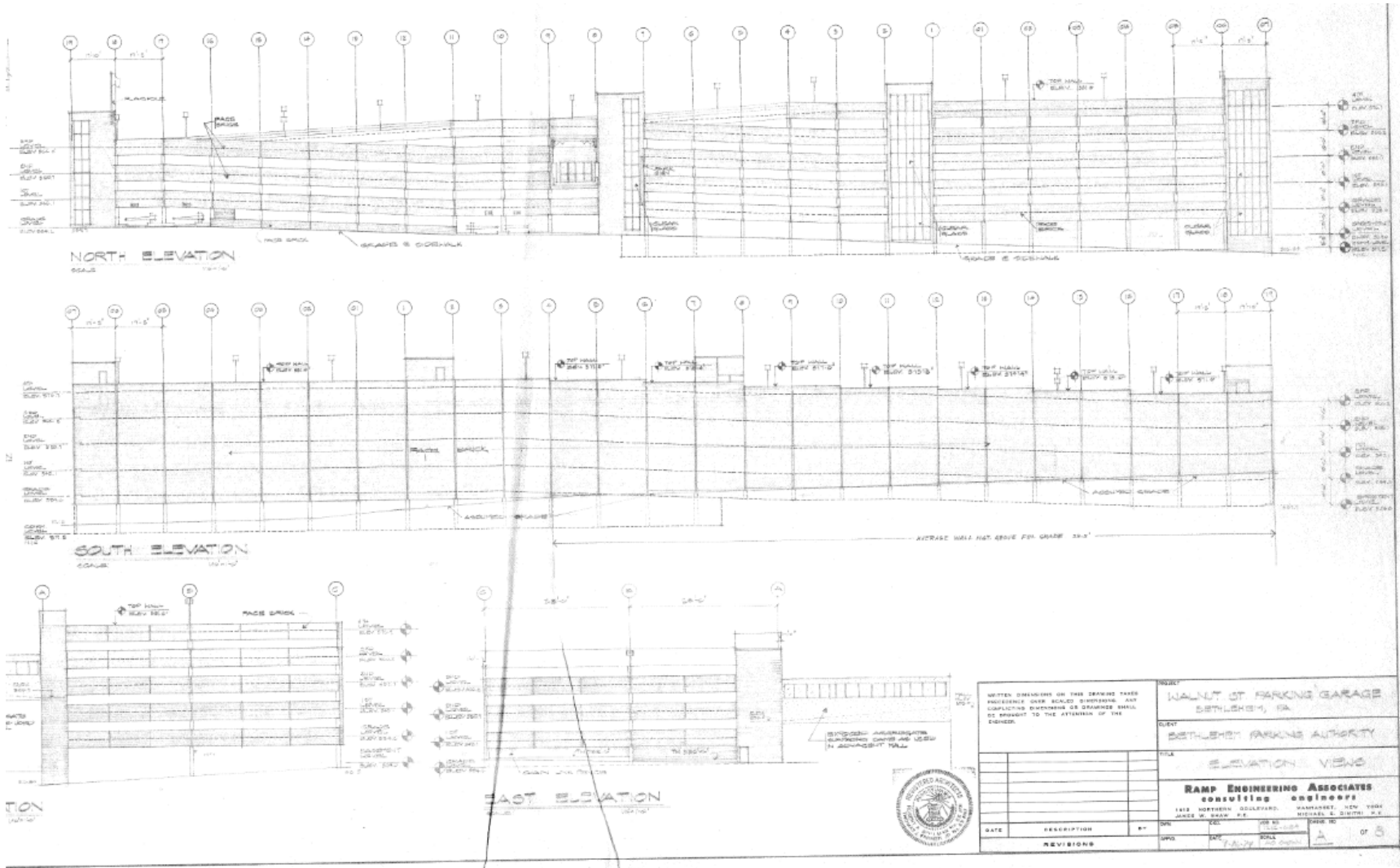
South Elevation



North Elevation / Pedestrian Bridge and Stair Tower

ELEVATIONS OF EXISTING GARAGE TO BE DEMOLISHED

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Why the demolition is necessary:

- A parking supply and demand assessment and site feasibility study shows that the proposed parking garage can be “right-sized” in design to provide less parking than the existing facility, but still provide adequate parking to support business activity in the area.
- The proposed smaller garage offers maximum flexibility for adjacent development.
- The BPA continues to spend significant capital funds to repair dangerous conditions, take portions of the structure out of service, and maintain safety.



Broken welds at brick façade support



Deteriorating structural steel framing & connections



Broken prestress strands at ceiling of concrete slab



Broken welds at brick façade supports

EXISTING GARAGE PHOTOGRAPHS

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Failed Facade Connection (Cracked Welds)



Deteriorating steel column



Deteriorating steel column



Shoring at Failed Steel Connection



Failed Steel Connection (Cracked Welds)



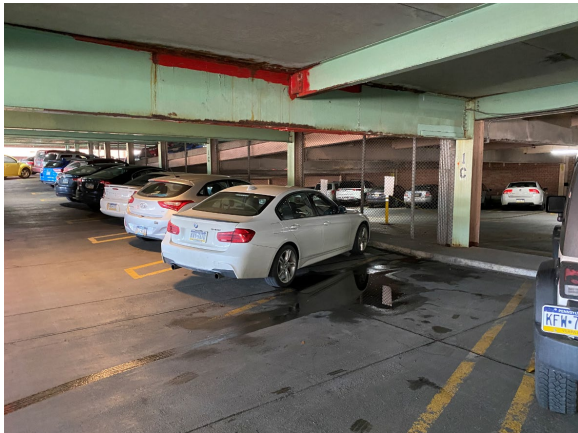
Deteriorating concrete deck

Why rehabilitation is not feasible:

- Parking Facility was constructed in early 1976, and is therefore 47 years old, and is a Non-Contributing Structure in the Historic District.
- The Parking Facility is in poor to dangerous condition and has reached the end of its service life expectancy.
- THA has been monitoring the garage on a quarterly basis and continues to discover portions of the structural steel frame, concrete floor slabs, and brick facade that develop into dangerous condition.
- Renovation of existing parking facility would cost more than demolition of the existing structure and construction of a new structure.



Deteriorating steel framing and connections



Example of deteriorating steel members



Example of deteriorating steel connection



Example of deteriorating steel connection

The BPA intends to award the project to the lowest “responsible” bidder that has the requisite qualifications, complete knowledge of project and clearly meets the following project requirements:

- A detailed demolition plan shall be provided that demonstrates how the Contractor intends to sequence the demolition that respects and exhibits a sensitivity to adjoining properties and owners.
- A logistics and traffic control plan shall be provided, that respects the adjoining properties and circulation on all streets. In addition, a plan of how the alleys, pedestrian pathways and connections shall be respected.
- A plan of protection for neighboring properties shall be implemented, in addition to site surveys of the properties that the Contractor believes could be affected by the demolition process.
- A detailed schedule of the demolition shall be provided, that meets or exceeds the milestone schedule.
- A vibration monitoring plan shall be implemented for adjacent properties for the entire duration of the demolition.
- A dust mitigation plan shall be implemented for the adjacent properties for the entire duration of the demolition.

ADJACENT RESIDENTIAL GARAGE DEMOLITION

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Residential garage to be demolished and rebuilt:

- On BPA lot, with easement. The residential garage was built as part of the construction of the public parking garage in 1976 and is therefore a Non-Contributing Structure.
- Difficult to protect and maintain safety during demolition; only 5' space at western end, and only 2'-6" space on eastern end
- Foundations of new parking garage will extend below the residential garage
- The replacement residential garage will have a design aesthetic that coordinates with the historic buildings in terms of shape and material.



Close proximity to rear of Walnut Street Parking Garage



South façade of residential 4-car garage



Interior; wood framing



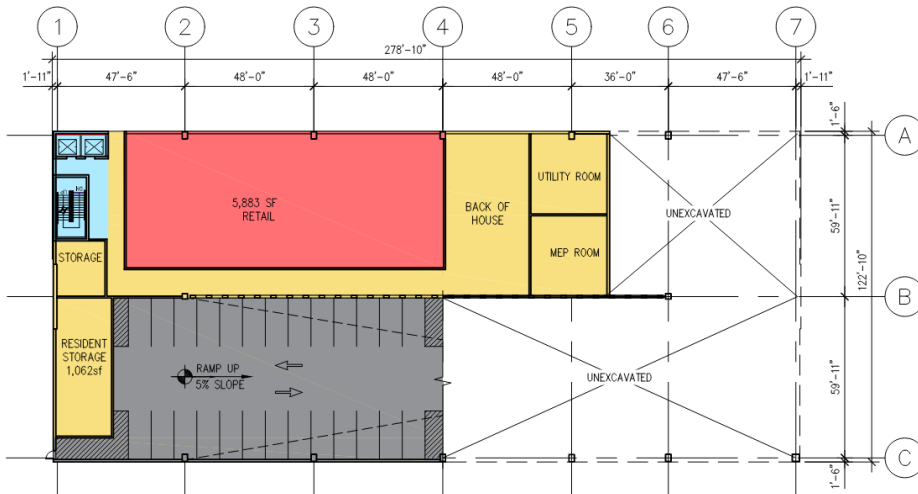
Shingle roof; close proximity

Proposal for new parking facility :

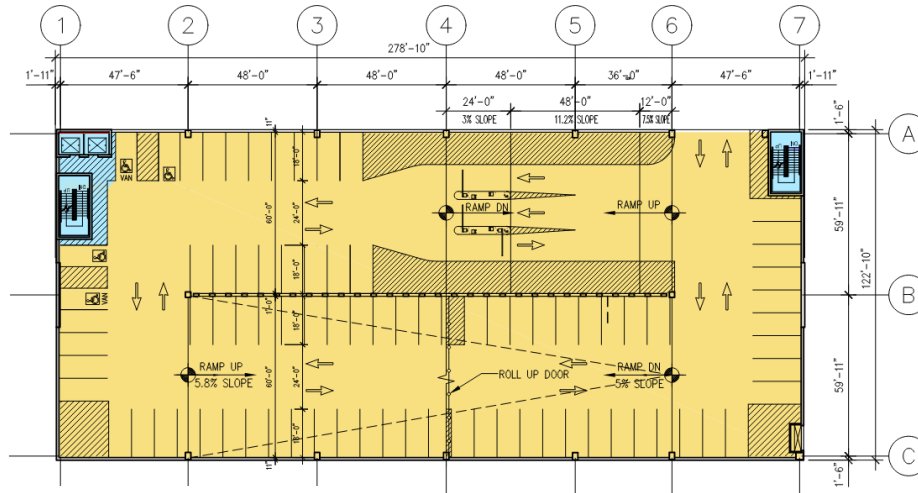
- New facility will be architecturally sensitive, and aesthetically enhanced
- Build a 591+ space “right-sized garage” that is less expensive to operate and maintain than the current facility
- Facility shall be sustainable (Parksmart Bronze certified)
- Provides an opportunity for revitalization and building block for future development
- Safe pedestrian connections to destinations
- Durable precast concrete structure with extended life
- Enclosed per code: mechanically ventilated and sprinklered
- Architecture: Horizontal lines, punched openings, rooflines, breakdown of massing, material choices
- MEP: enclosed garage requiring mechanical ventilation and sprinklers, and other code impacts
- Site: Curb cuts and vehicular entry/exit locations on Walnut Street (existing location)
- Program: Recognition of property ownership in parking areas, retail operational aspects
- Schedule: Timely demolition of the existing structure due to condition, but also not too far ahead of the construction of the new parking structure

PROPOSED BUILDING PLAN: GARAGE

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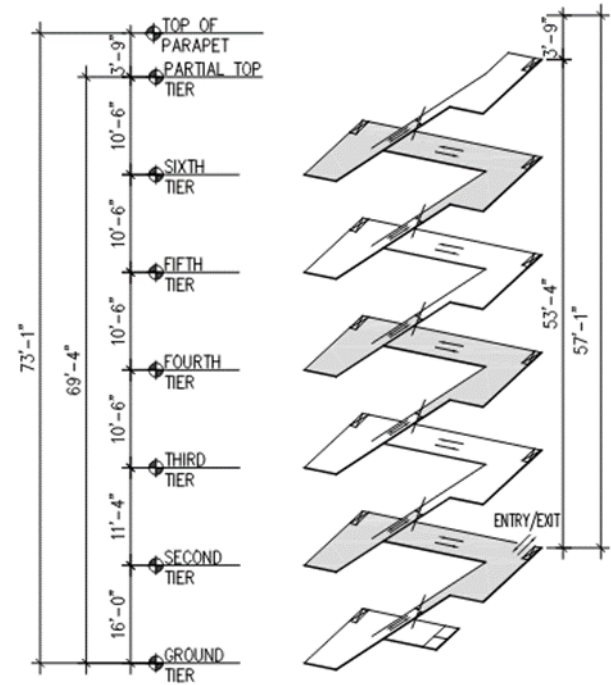


GROUND TIER PLAN



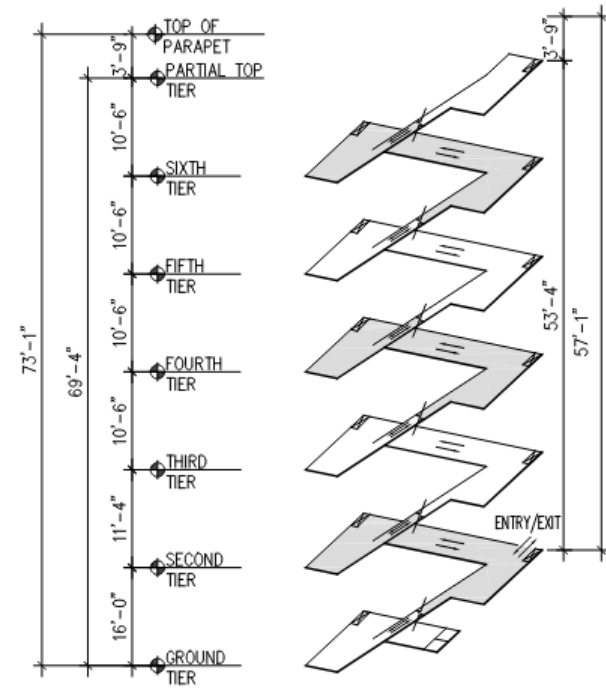
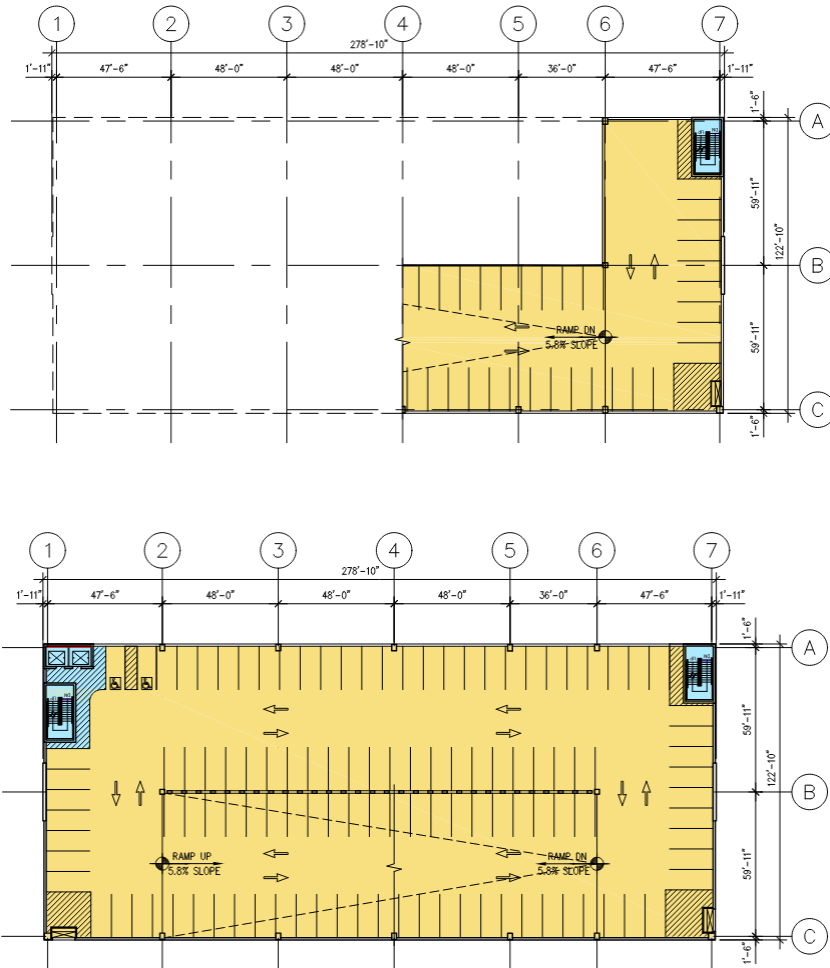
SECOND TIER PLAN

PARKING SPACE TABULATION				
LEVEL	STANDARD	ACCESSIBLE	VAN ACCESSIBLE	TOTAL
Top	32	0	0	32
6th	111	2	0	113
5th	111	2	0	113
4th	111	2	0	113
3rd	111	2	0	113
2nd	77	2	2	81
Ground	26	0	0	26
TOTAL	579	10	2	591



PROPOSED BUILDING PLAN: GARAGE

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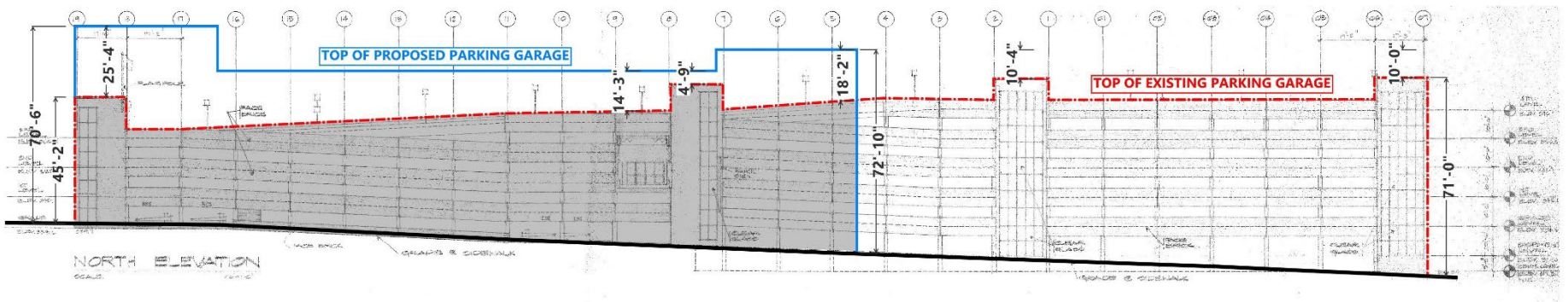
PROPOSED BUILDING MASSING: GARAGE

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Overlay on the elevations of the existing garage :

- Refer to elevation diagram below for overall heights, and height differentials
- Proposed retail component (storefront) on western (right) end of Walnut Street façade (not shown)
- Minimal openings on southern façade (rear of garage) facing residential neighborhood (block light leakage, provide fire separation)
- Solid western façade abutting proposed development





Intersection of Main Street and Market Street



Intersection of Main Street and Market Street



Intersection of Main Street and Market Street



Intersection of Main Street and Market Street



- Storefronts have large sheets of glass, and the systems are primarily composed of glass and decorative metalwork
- Storefronts are often recessed within an alcove, especially at the Entrance Door

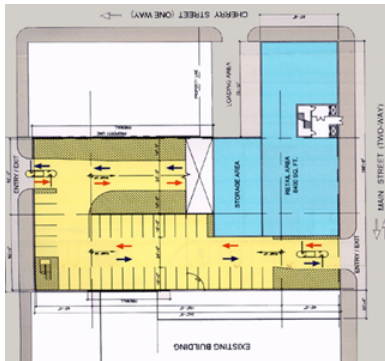
- The Storefront Cornice provides an “eyebrow” at the ground floor to maintain pedestrian scale
- Above the Display Windows, the Storefront system typically has a Transom Window with a Signage Marquee or Awning



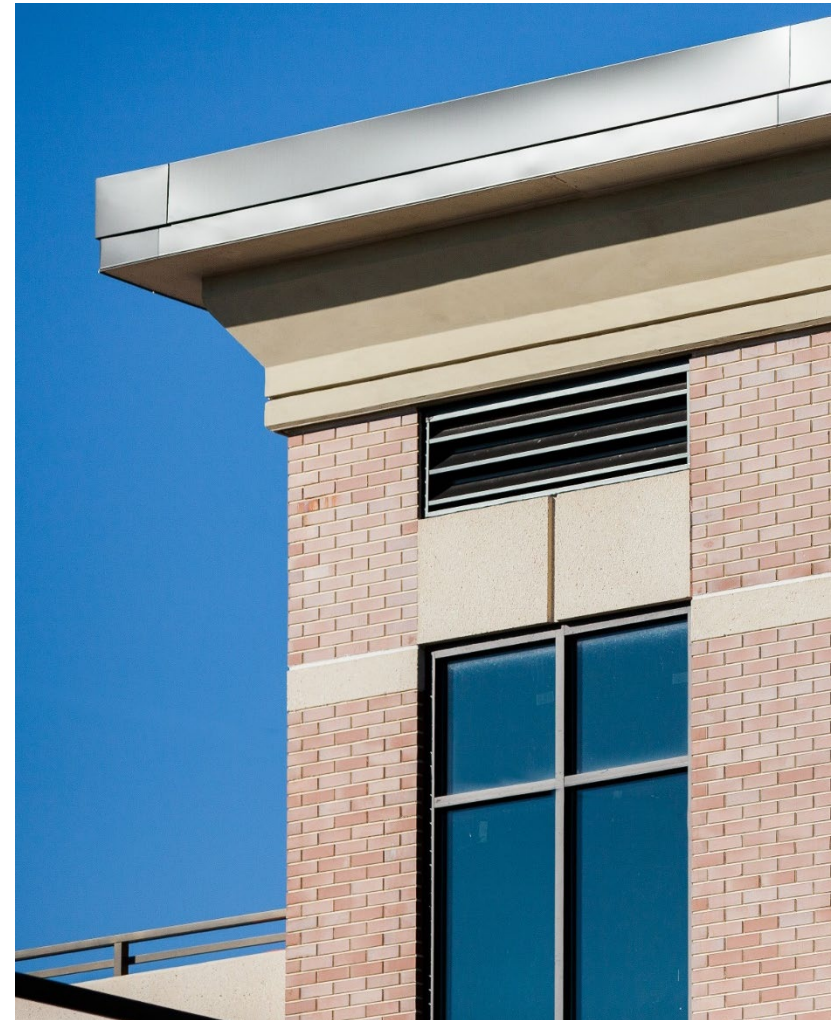
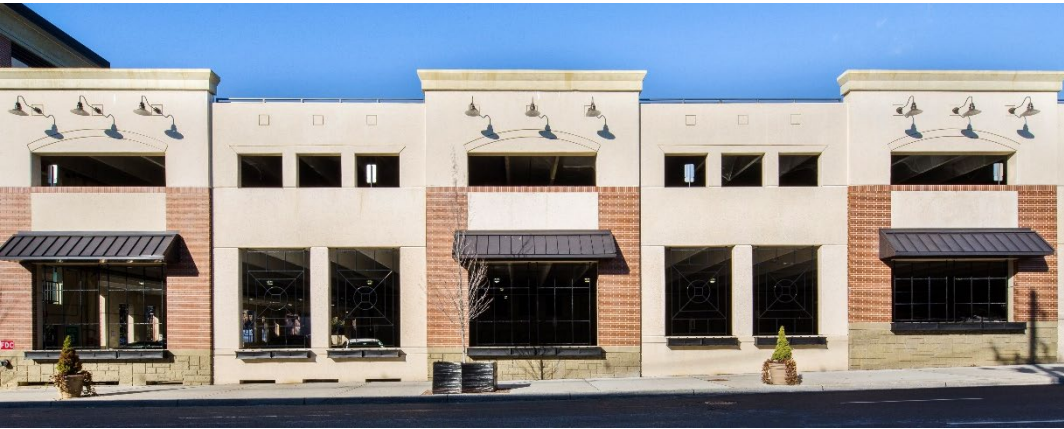
- Upper Floors are primarily solid, more “simple”/less textured, with punched openings
- Brick or stone is primary building material for Upper Floors, while the Ground floor “base” may have a slightly more textured material

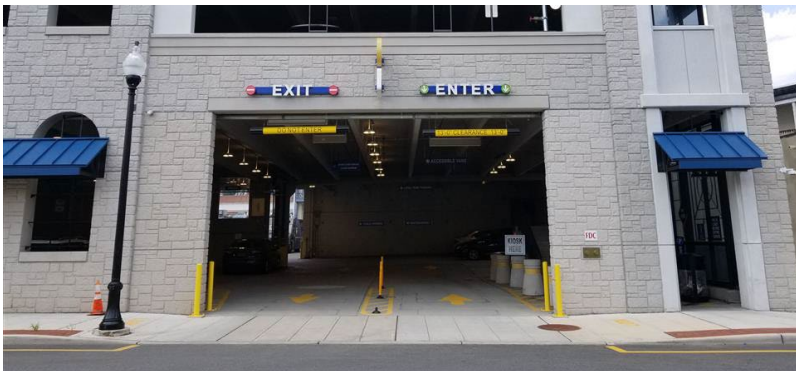
- Ornamental Building Cornices: Projected moldings / decorative roofline providing a visual cap
- Cascading floor level and roofline due to grade changes















POTENTIAL PROJECT SCHEDULE

August 2, 2023

TIMELINE	TASK
3/1 to 6/30/2023	Complete Demolition Bid Docs
5/15 to 7/6/2023	NPDES
7/5/2023	HARB Submission (4 weeks prior to meeting)
8/2/2023	HARB Meeting
9/6/2023	HARB Meeting
Mid to Late September 2023	Anticipated HARB Certificate of Appropriateness
September thru October 2023	Demolition Permit/Bidding/Award/Contracts
November 2023	HARB Submission for New Build/ HARB Meeting
January to May 2024	Garage Demolition & Contract Closeout
June 2024	Commence Construction of New Garage

Thank you!!

