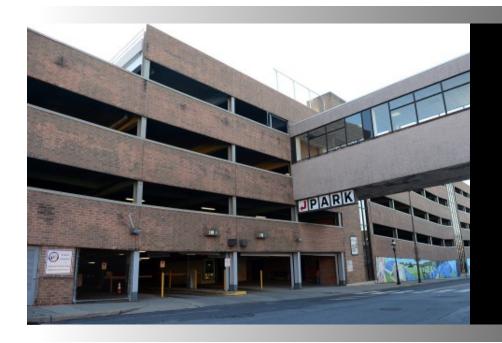
City of Bethlehem, Pennsylvania

APPLICATION FOR CERTIFICATE OF APPROPRIATENESS (COA)

Building address		reet, Bethlehem, PA	TOATE OF ALL	I KOI KIA	(TENESS)	COA)		
Owner of building	Bethlehem Parking Authority (Steve Fernstrom,,ED)			Phone	none			
Owner's email & 1			5 West North St., Be	-	18018	-		
		Carrigan)	Phone:					
Applicant THA Consulting, Inc. (Kevin Carrigan) Phone: Applicant's email & mailing address 470 Norristown Road, Blue Bell, PA 19422								
rippiicule s <u>cinan</u>	Street and Numb		City		State	Zip Code		
	4 DDI 161 4 N	E MALICON ASSESSED	MEEDING E	n alar	TO DE 111	•		
USE THE CHI	APPLICANT MUST ATTEND MEETING FOR CASE TO BE HEARD. USE THE CHECKLIST ON THE BACK OF THIS APPLICATION TO ENSURE YOUR SUBMISSION IS							
COMPLETE.								
Application form, photographs, and drawings must be submitted (see attached for deadline) prior to the regular scheduled meeting in order to be placed on the agenda for the next meeting.								
1. PHOTOGRAPH	S - Photographs o	of your building and	neighboring buildi	ings <u>must</u>	ассотраг	v your application.		
2. TYPE OF WOR	K PROPOSED -	Check all that apply	y. Please bring any	y samples o	r manufactur	es specifications for		
products you will	use in this projec	t.						
Trim and de	corative woodwo	rk	\$	Skylights				
Siding and M	Siding and Masonry			Metal work				
Roofing, gu	Roofing, gutter and downspoutLight fixtures							
Windows, d	Windows, doors, and associated hardware Signs							
Storm windo	Storm windows and storm doors X Demolition							
Shutters and	l associated hardw	vare		Other				
Paint (Subm	it color chips – H	(ARB only)						
3. DRAWINGS OF I		_						
	<u>EN (10) COPIE</u>	ES OF DRAWING	S, PHOTOGRA	PHS, APF	<u>'LICATIOI</u>	N FORM, AND ANY		
SPECIFICATIONS								
Alteration, renovation, restoration (1/4 or 1/8"=1'0" scale drawings required IF walls or openings altered.)								
New addition (1/4" or 1/8"==1'0" scale drawings: elevations, floor plans, site plan)								
New building or structure (1/4" or 1/8"=1'0" scale drawings: elevations, floor plans, site plan) X Demolition, removal of building features or building (1/4" or 1/8"=1'0" scale drawings; elevation of remaining site								
Demonstron, removal or bunding readeres of bunding (1/4 of 1/8 -1 of scale drawings, elevation of remaining site								
and site plan) A scale drawing, with an elevation view, is required for all sign submittals								
A scale drav	ving, with an elev	ation view, is require	ed for all sign subr	nittals		.,		
4. DESCRIBE PRO	DJECT – Describ	e any work checked	in #2 and #3 abov	e. Attach a	dditional she	ets as needed.		
4. DESCRIBE PROJECT — Describe any work checked in #2 and #3 above. Attach additional sheets as needed. Existing Parking Structure, originally constructed in 1976, has reached the end of it's life expectancy. Any work required to bring it back to good condition and extend its life would be extensive and expensive. Work will consist of demolishing the 5 tier garage, which is approximately 476 feet long by 116' wide by 57' to the top deck. The existing garage consists of a steel structural frame, with concrete decks, and a concrete facade with thin brick. Full demolition package is attached.								
5. APPLICANT'S S	GIGNATURE	Kevin Carrigar	, PE, SE		DATE: 6/	30/2023		
OWNER'S SIGN	ATURE				_DATE <u>:</u>			



Bethlehem Parking Authority

33 Walnut Street Parking Garage Replacement

Historic / Architectural Review Board Meeting Existing Garage Demolition

August 2, 2023



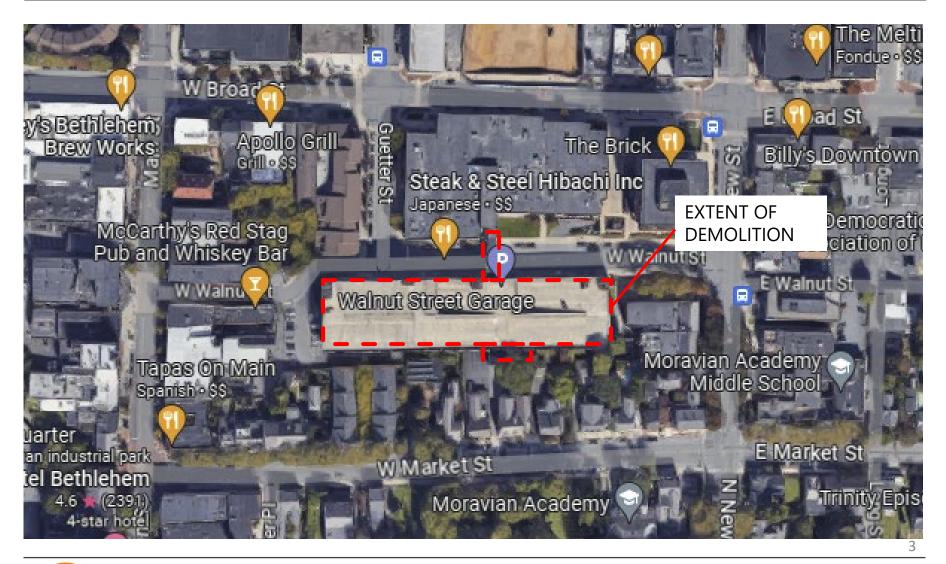


- Demolition of existing parking structure located at 33 West Walnut Street, Bethlehem, PA
- Parking facility will include core/shell design for approximately 6,000 sf of amenity space at ground tier
- West side of site future development
- Zoning District: CB Central Business District, Downtown Bethlehem Historic District



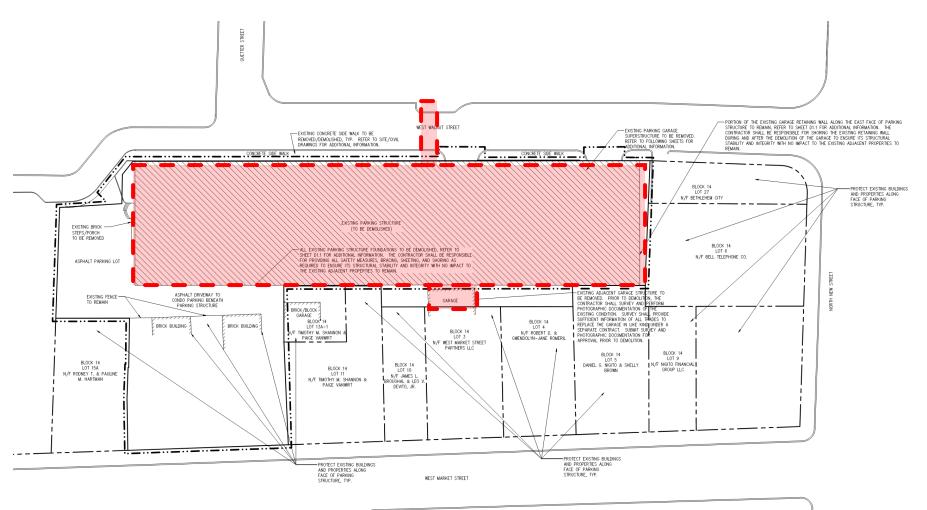












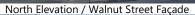




EXISTING CONDITIONS

August 2, 2023











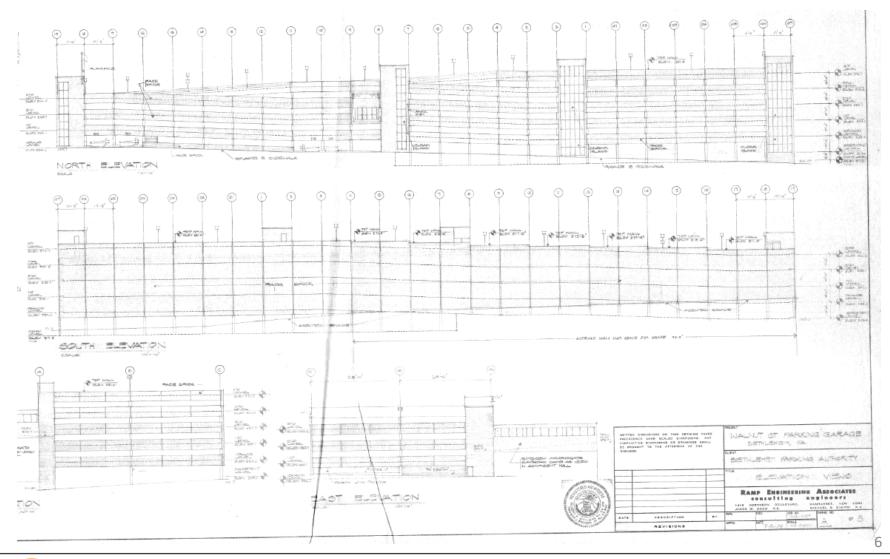


North Elevation / Pedestrian Bridge and Stair Tower



East Elevation









DEMOLITION NARRATIVE

Why the demolition is necessary:

- A parking supply and demand assessment and site feasibility study shows that the
 proposed parking garage can be "right-sized" in design to provide less parking
 than the existing facility, but still provide adequate parking to support business
 activity in the area.
- The proposed smaller garage offers maximum flexibility for adjacent development.
- The BPA continues to spend significant capital funds to repair dangerous conditions, take portions of the structure out of service, and maintain safety.



Broken welds at brick façade support



Deteriorating structural steel framing $\&\ connections$



Broken prestress strands at ceiling of concrete slab



Broken welds at brick façade supports





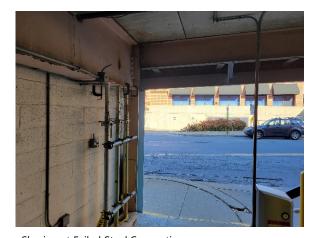


Failed Facade Connection (Cracked Welds)



Deteriorating steel column





Shoring at Failed Steel Connection



Failed Steel Connection (Cracked Welds)



Deteriorating concrete deck





DEMOLITION NARRATIVE August 2, 2023

Why rehabilitation is not feasible:

- Parking Facility was constructed in early 1976, and is therefore 47 years old, and is a Non-Contributing Structure in the Historic District.
- The Parking Facility is in poor to dangerous condition and has reached the end of its service life expectancy.
- THA has been monitoring the garage on a quarterly basis and continues to discover portions of the structural steel frame, concrete floor slabs, and brick facade that develop into dangerous condition.
- Renovation of existing parking facility would cost more than demolition of the existing structure and construction of a new structure.



Deteriorating steel framing and connections



Example of deteriorating steel members



Example of deteriorating steel connection



Example of deteriorating steel connection







DEMOLITION PLAN

August 2, 2023

The BPA intends to award the project to the lowest "responsible" bidder that has the requisite qualifications, complete knowledge of project and clearly meets the following project requirements:

- A detailed demolition plan shall be provided that demonstrates how the Contractor intends to sequence the demolition that respects and exhibits a sensitivity to adjoining properties and owners.
- A logistics and traffic control plan shall be provided, that respects the adjoining properties and circulation on all streets. In addition, a plan of how the alleys, pedestrian pathways and connections shall be respected.
- A plan of protection for neighboring properties shall be implemented, in addition to site surveys of the properties that the Contractor believes could be affected by the demolition process.
- A detailed schedule of the demolition shall be provided, that meets or exceeds the milestone schedule.
- A vibration monitoring plan shall be implemented for adjacent properties for the entire duration of the demolition.
- A dust mitigation plan shall be implemented for the adjacent properties for the entire duration of the demolition.





Residential garage to be demolished and rebuilt:

- On BPA lot, with easement. The residential garage was built as part of the construction of the public parking garage in 1976 and is therefore a Non-Contributing Structure.
- Difficult to protect and maintain safety during demolition; only 5' space at western end, and only 2'-6" space on eastern end
- Foundations of new parking garage will extend below the residential garage
- The replacement residential garage will have a design aesthetic that coordinates with the historic buildings in terms of shape and material.



Close proximity to rear of Walnut Street Parking Garage



South façade of residential 4-car garage



Interior; wood framing



Shingle roof; close proximity



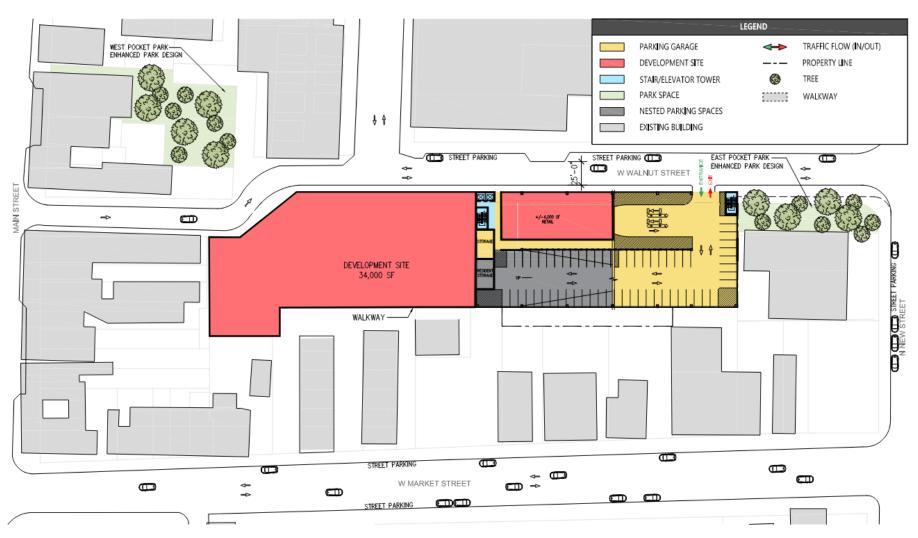


Proposal for new parking facility:

- · New facility will be architecturally sensitive, and aesthetically enhanced
- Build a 591+ space "right-sized garage" that is less expensive to operate and maintain than the current facility
- Facility shall be sustainable (Parksmart Bronze certified)
- Provides an opportunity for revitalization and building block for future development
- Safe pedestrian connections to destinations
- Durable precast concrete structure with extended life
- · Enclosed per code: mechanically ventilated and sprinklered
- Architecture: Horizontal lines, punched openings, rooflines, breakdown of massing, material choices
- MEP: enclosed garage requiring mechanical ventilation and sprinklers, and other code impacts
- Site: Curb cuts and vehicular entry/exit locations on Walnut Street (existing location)
- Program: Recognition of property ownership in parking areas, retail operational aspects
- Schedule: Timely demolition of the existing structure due to condition, but also not too far ahead of the construction of the new parking structure

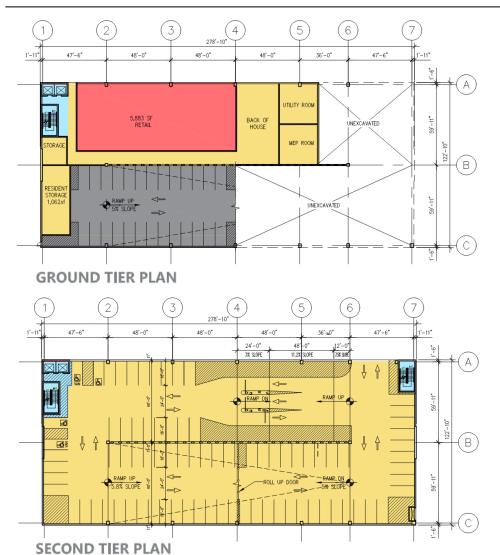




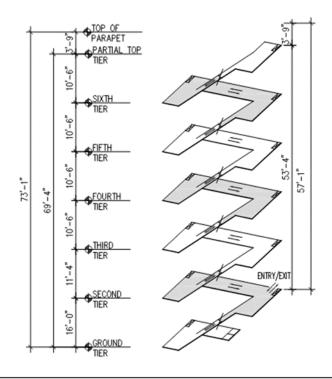






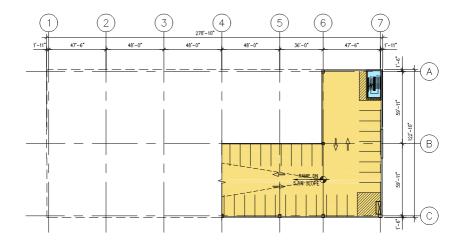


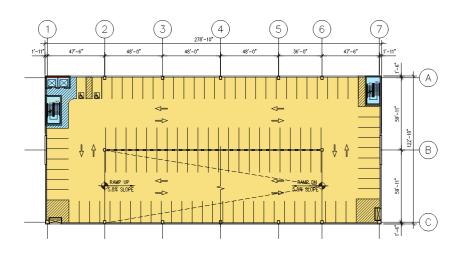
PARKING SPACE TABULATION							
LEVEL	STANDARD	ACCESSIBLE	VAN ACCESSIBLE	TOTAL			
Тор	32	0	0	32			
6th	111	2	0	113			
5th	111	2	0	113			
4th	111	2	0	113			
3rd	111	2	0	113			
2nd	77	2	2	81			
Ground	26	0	0	26			
TOTAL	579	10	2	591			

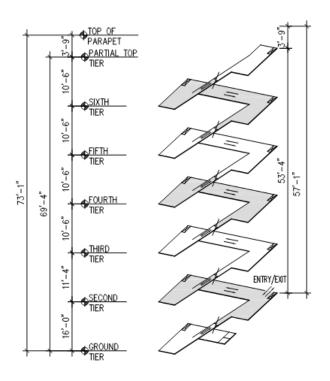
















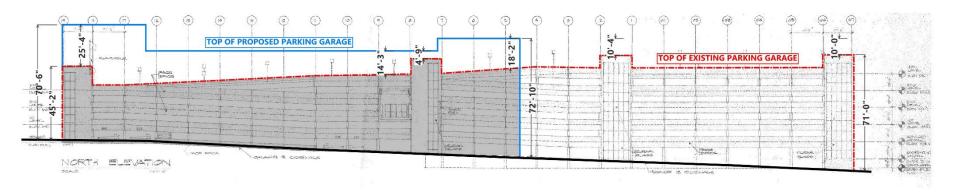






Overlay on the elevations of the existing garage:

- Refer to elevation diagram below for overall heights, and height differentials
- Proposed retail component (storefront) on western (right) end of Walnut Street façade (not shown)
- Minimal openings on southern façade (rear of garage) facing residential neighborhood (block light leakage, provide fire separation)
- Solid western façade abutting proposed development







SURROUNDING CONTEXT



Intersection of Main Street and Market Street



Intersection of Main Street and Market Street



Intersection of Main Street and Market Street

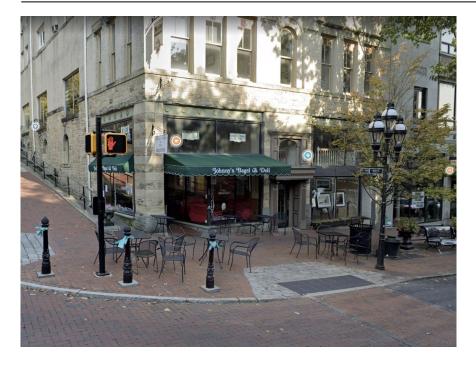


Intersection of Main Street and Market Street





SURROUNDING CONTEXT





- Storefronts have large sheets of glass, and the systems are primarily composed of glass and decorative metalwork
- Storefronts are often recessed within an alcove, especially at the Entrance Door
- The Storefront Cornice provides an "eyebrow" at the ground floor to maintain pedestrian scale
- Above the Display Windows, the Storefront system typically has a Transom Window with a Signage Marquee or Awning









- Upper Floors are primarily solid, more
 "simple"/less textured, with punched openings
- Brick or stone is primary building material for Upper Floors, while the Ground floor "base" may have a slightly more textured material
- Ornamental Building Cornices: Projected moldings / decorative roofline providing a visual cap
- Cascading floor level and roofline due to grade changes











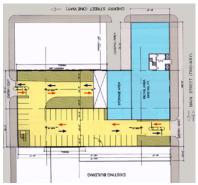














































































TIMELINE	TASK			
3/1 to 6/30/2023	Complete Demolition Bid Docs			
5/15 to 7/6/2023	NPDES			
7/5/2023	HARB Submission (4 weeks prior to meeting)			
8/2/2023	HARB Meeting			
9/6/2023	HARB Meeting			
Mid to Late September 2023	Anticipated HARB Certificate of Appropriateness			
September thru October 2023	Demolition Permit/Bidding/Award/Contracts			
November 2023	HARB Submission for New Build/ HARB Meeting			
January to May 2024	Garage Demolition & Contract Closeout			
June 2024	Commence Construction of New Garage			





SUMMARY

Thank you!!



